

Sylvania Interchange Study

City Council Presentation

June 18, 2018

PID 105889

LUC-US23-11.75



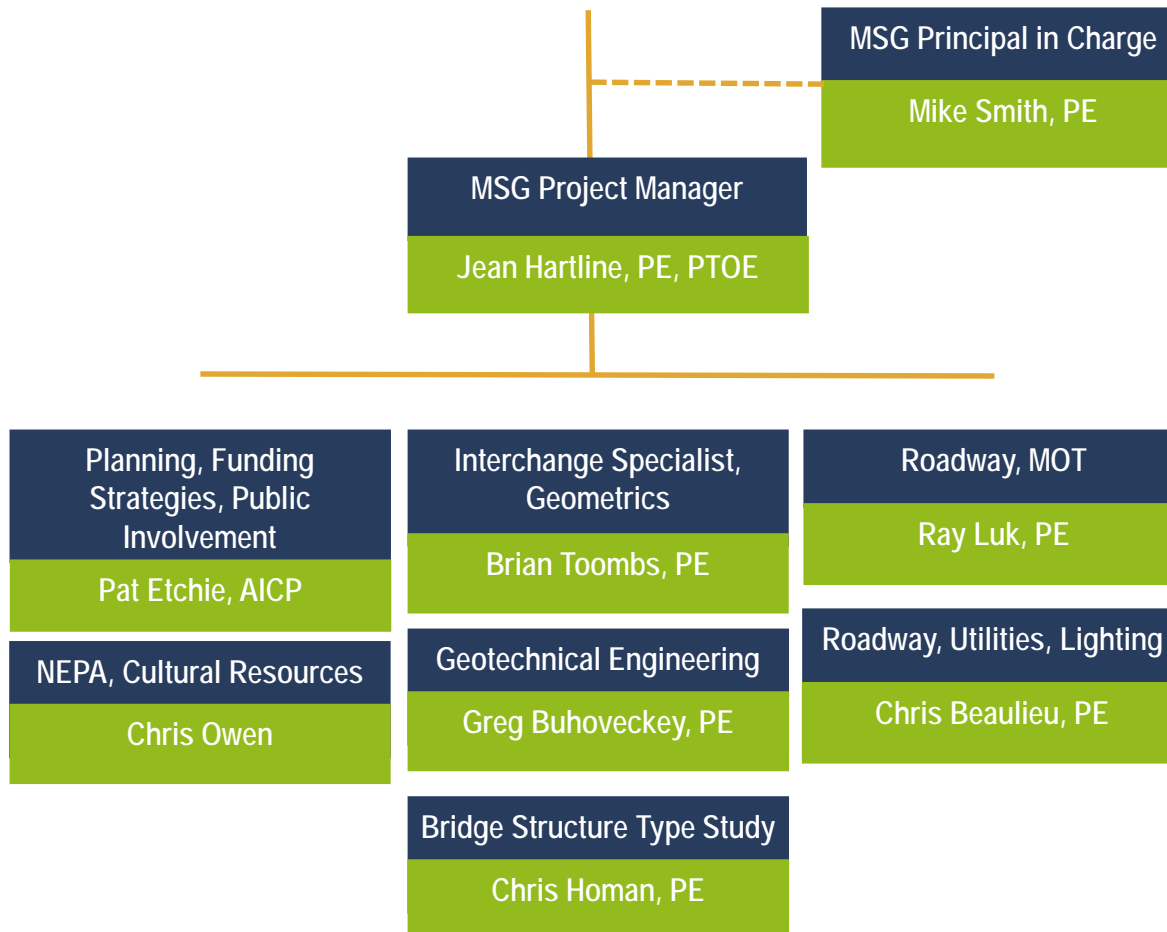
Agenda

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1. Project Team
2. Study Area
3. ODOT Process
4. Project Challenges
5. Project Purpose and Need
6. Environmental Issues
7. Study Process and Schedule
8. Public involvement
9. Interchange Alternatives (Eliminated and Feasible)
10. Next Steps

Project Team

The Mannik & Smith Group, Inc. (MSG)
Burgess & Niple, Inc. (B&N)
Burton Planning Services (BPS)



Study Area: Interchange and Monroe Corridor

Includes:

- Interchange
- US 23 North and South of Monroe
- Monroe from Haroun Road to Tireman/Rite Aid/5/3 Bank Signal
- Alexis Road from Monroe to Acres



Following the ODOT Process



- Better for future federal and/or state funding
- Engages ODOT as a partner in the process
- Ultimately a cost savings
- Requires:
 - Public Involvement
 - Environmental Compliance
 - Design Standards
 - Ultimately required for the FHWA Interchange Modification Process

Project Challenges



Local Street System

- Monroe Street Congestion
- Crashes (Safety)
- Monroe Street Close Signal Spacing (east and west of US 23)
- Complex Signalized Intersections
- Glasgow Road – isolated dead end road
- Interchange is “Gateway” to the City
- Need for non-motorized facilities and connectivity
- Access Management
- Stakeholders (Property owners)

Interstate System

- Non-Conventional Interchange Configuration
- Tight Ramp Curves
- Ramp Congestion at Monroe Street Signals
- Adjacent ODOT Programmed Projects

Funding

- Federal State Funding is a Process
- Project Phasing may benefit funding process

Adjacent Projects

US 23 Noise Walls (ODOT PID 103647)

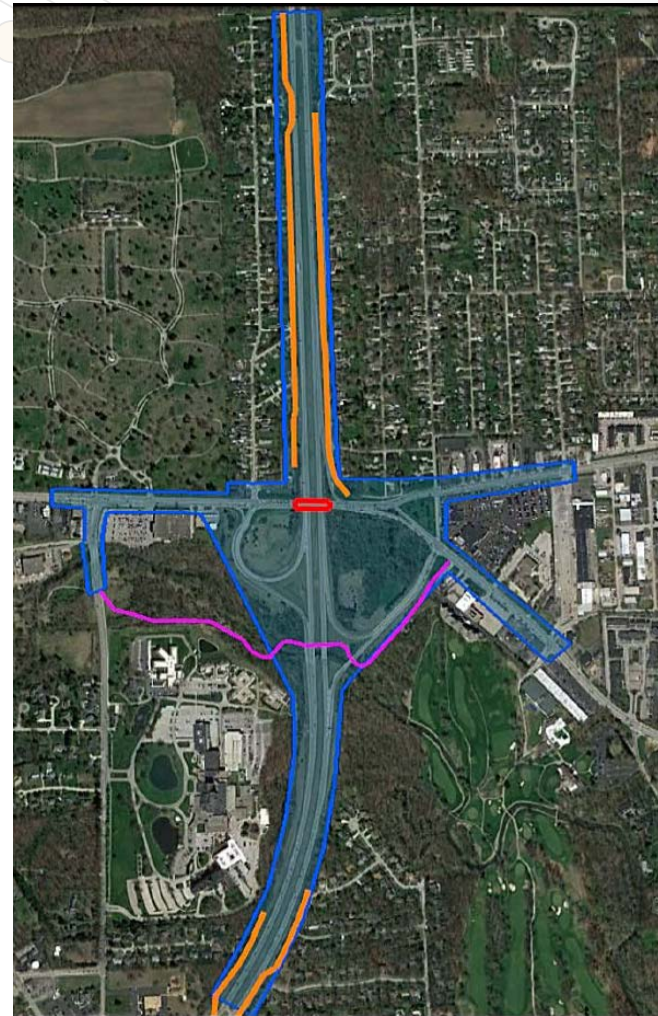
- Currently in design
- \$6Million
- 2020 Construction

Monroe Street (SR 51) Bridge (PID 96010)

- Currently in Design
- \$3Million
- 2021 Construction

US 23 NB Off Ramp (ODOT PID 101333)

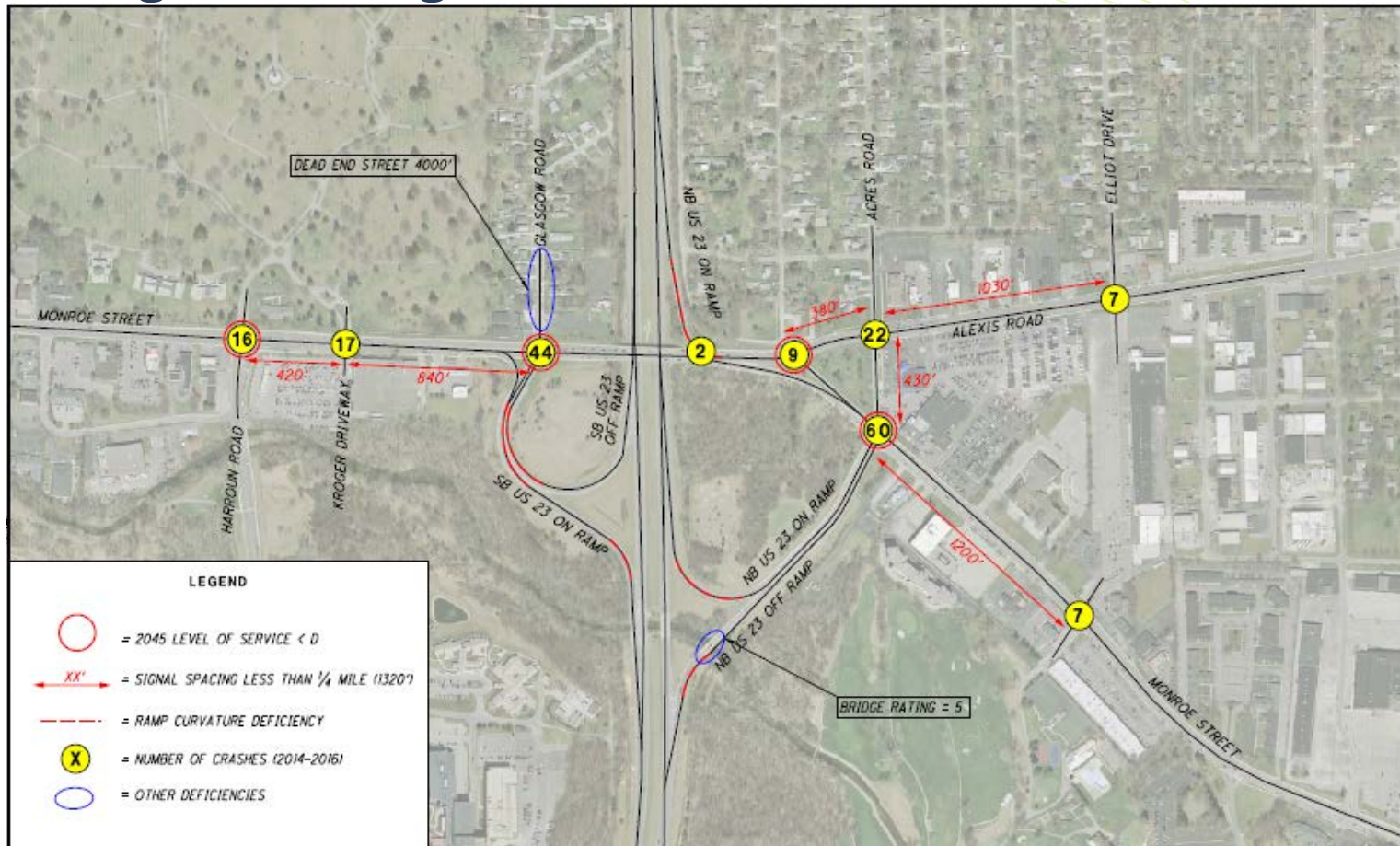
- Bridge Rating 5/10
- ODOT: “Replace and Re-align” Ramp/Bridge
- Not in Design
- \$6.1Million (budgetary estimate)
- 2024 Construction (estimate)



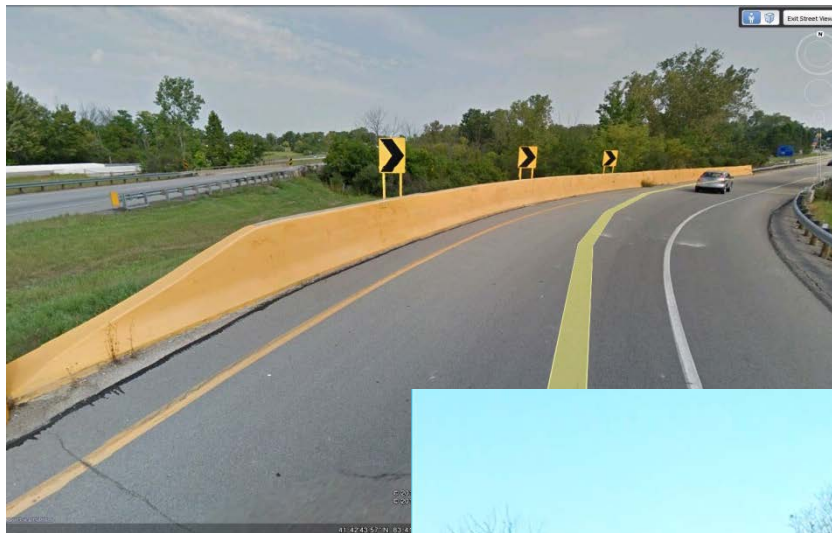
Timely decisions will benefit these projects and save taxpayer money.

Project Purpose and Need

- Safety concerns on Ramps and Intersections
- Congestion at Signalized Intersections



Some crashes not reported



Google Earth recent view:
Recently painted barrier

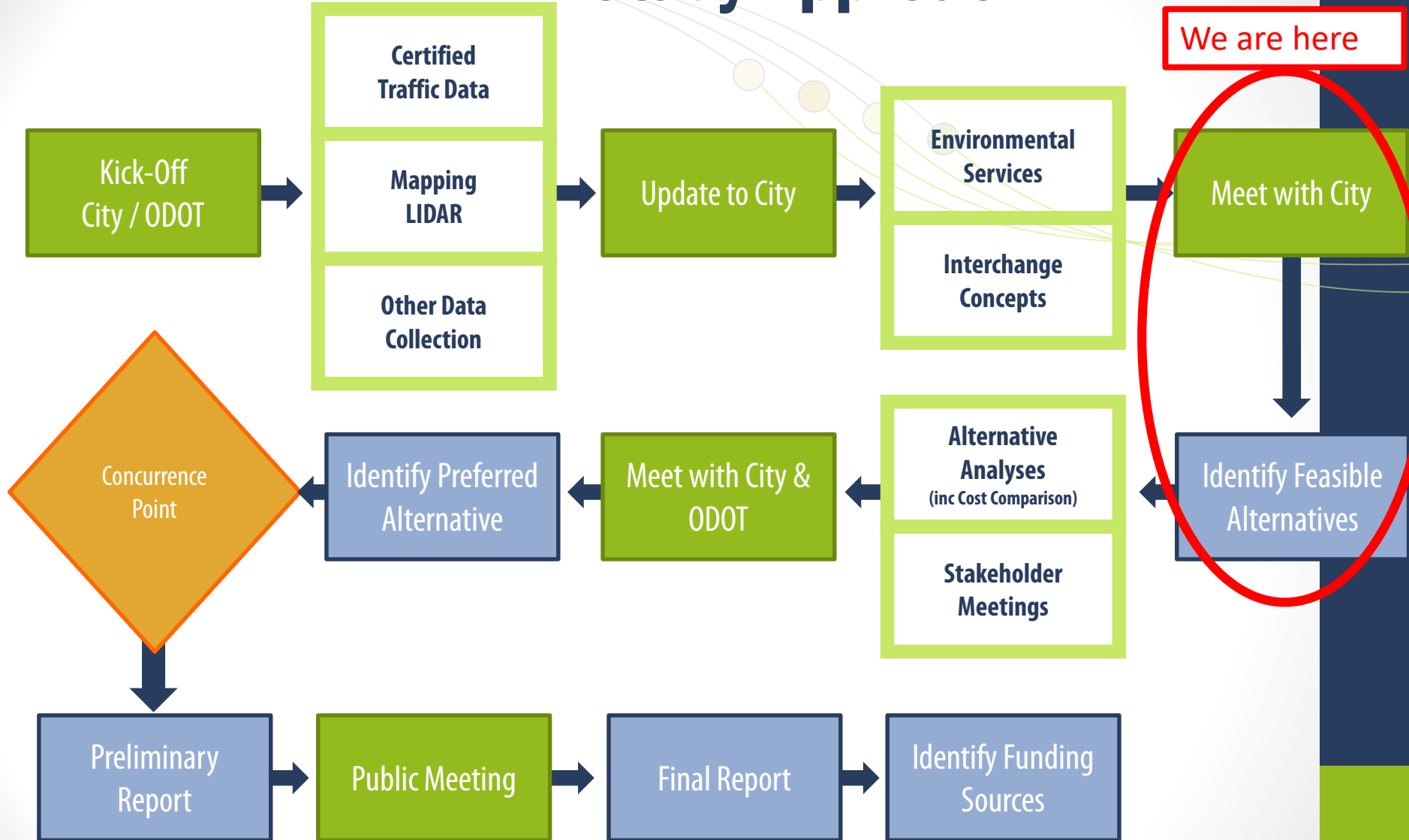
Photo February:
Several barrier strikes



- slope failures)



Study Approach:



Study Schedule

| Task | Month | | | | | | | | | | | |
|--|----------|----------|----------|----------|------------|----------|-----------|-----------|----------|-----------|----------|----------|
| | Nov 2017 | Jan 2018 | Feb 2018 | Mar 2018 | April 2018 | May 2018 | June 2018 | July 2018 | Aug 2018 | Sept 2018 | Oct 2018 | Nov 2018 |
| Project Scoping | | | | | | | | | | | | |
| Project Kick Off | | | | | | | | | | | | |
| Certified Traffic | | | | | | | | | | | | |
| Mapping | | | | | | | | | | | | |
| Data Collection | | | | | | | | | | | | |
| Update City/ ODOT Coordination | | | | | | | | | | | | |
| Environmental Screening | | | | | | | | | | | | |
| Meet with the City | | | | | | | | | | | | |
| Identify Feasible Alternatives | | | | | | | | | | | | |
| Meet with City and ODOT | | | | | | | | | | | | |
| Meet with City Council | | | | | | | | | | | | |
| Alternative Analysis | | | | | | | | | | | | |
| Identify Preferred Alternative | | | | | | | | | | | | |
| Draft Report | | | | | | | | | | | | |
| Public Meeting (inc 30 day comment period) | | | | | | | | | | | | |
| Final Report | | | | | | | | | | | | |
| Funding Sources | | | | | | | | | | | | |

We are here

Public Involvement

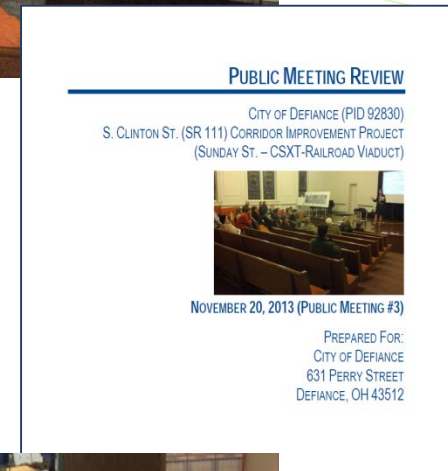
Stakeholder Involvement

Identify Key Stakeholders

- Ciao! Restaurant
 - Glasgow Road Residents
 - Toledo Memorial Park Cemetery
 - Dave White Auto-Dealership
 - BP Barneys
 - Kroger
 - Flower Hospital
 - City Services (Fire/EMS, Police, Parks)
 - Corridor Restaurants/Businesses
 - Others
- Consider Stakeholder Workshop
 - Identify their short term and long range plans
 - Understand their key needs

Public Meeting

- Required for the NEPA Process
- Documentation/Comments is vital
- October 2018 Date for this project



Interchange Concepts: Eliminated and Feasible

Minimum Alternatives:

- No Build
- Retain existing folded diamond with reconstructed loops and lane additions

Reconfigurations

- Diamond or Tight Diamond (with/without roundabouts) TUDI
- Single Point Urban Interchange (SPUI Central Avenue)
- Diverging Diamond Interchange
- Hybrid Configurations

TUDI Defiance - MSG



DDI Columbus - B&N



SPUI Central - MSG/B&N



What Eliminates an Interchange Alternative?



- Primarily: Does not meet Purpose and Need
 - Does not improve capacity operations (measured in terms of level of service)
 - Does not address crash patterns
 - Geometric Deficiencies (i.e. ramp curvatures)
 - Signal Spacing
- Right-of-way Impacts
- Cost
- Environmental Impacts

Eliminated - Diverging Diamond Interchange

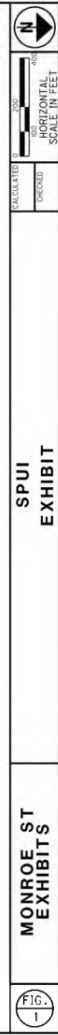




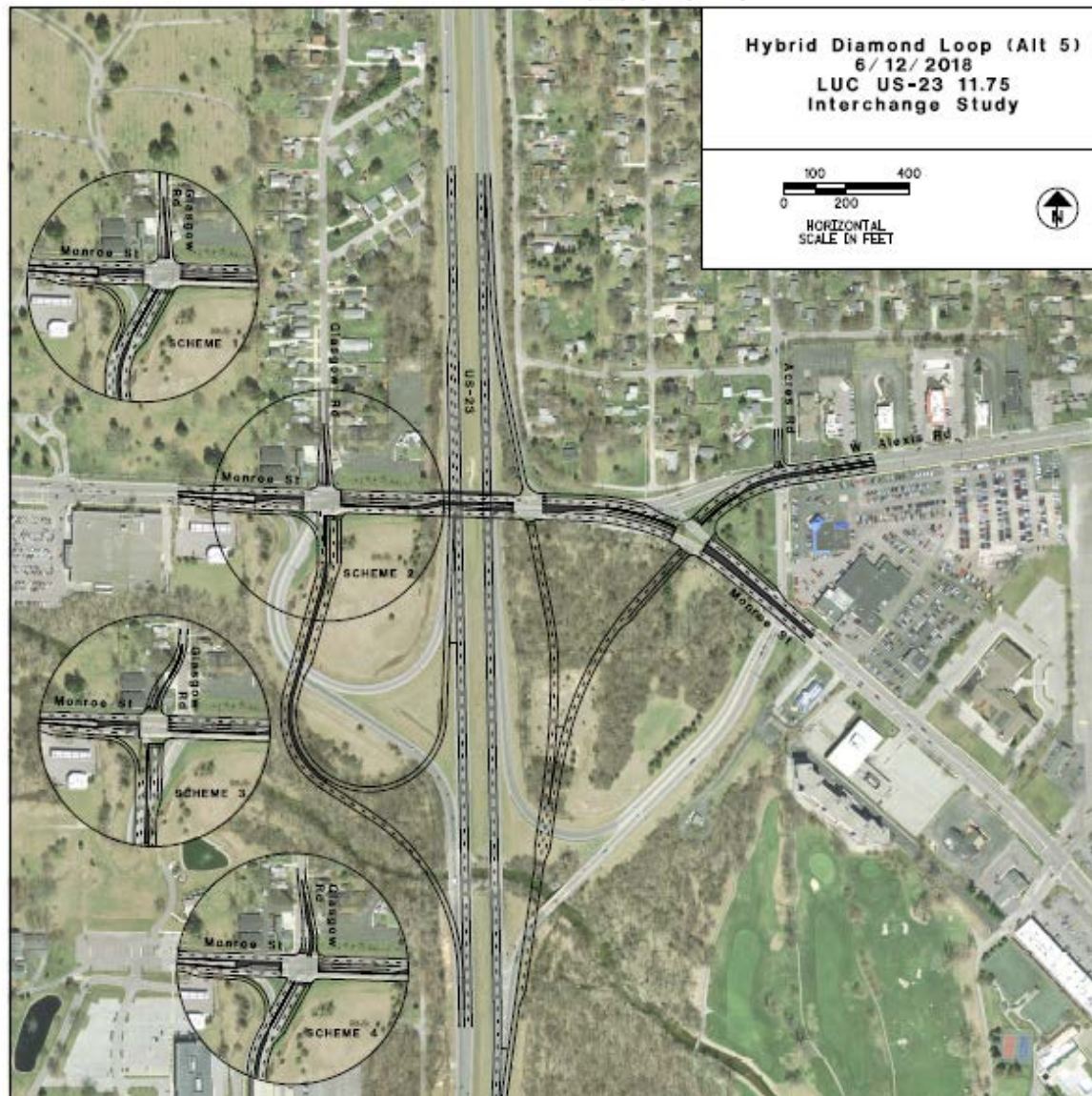




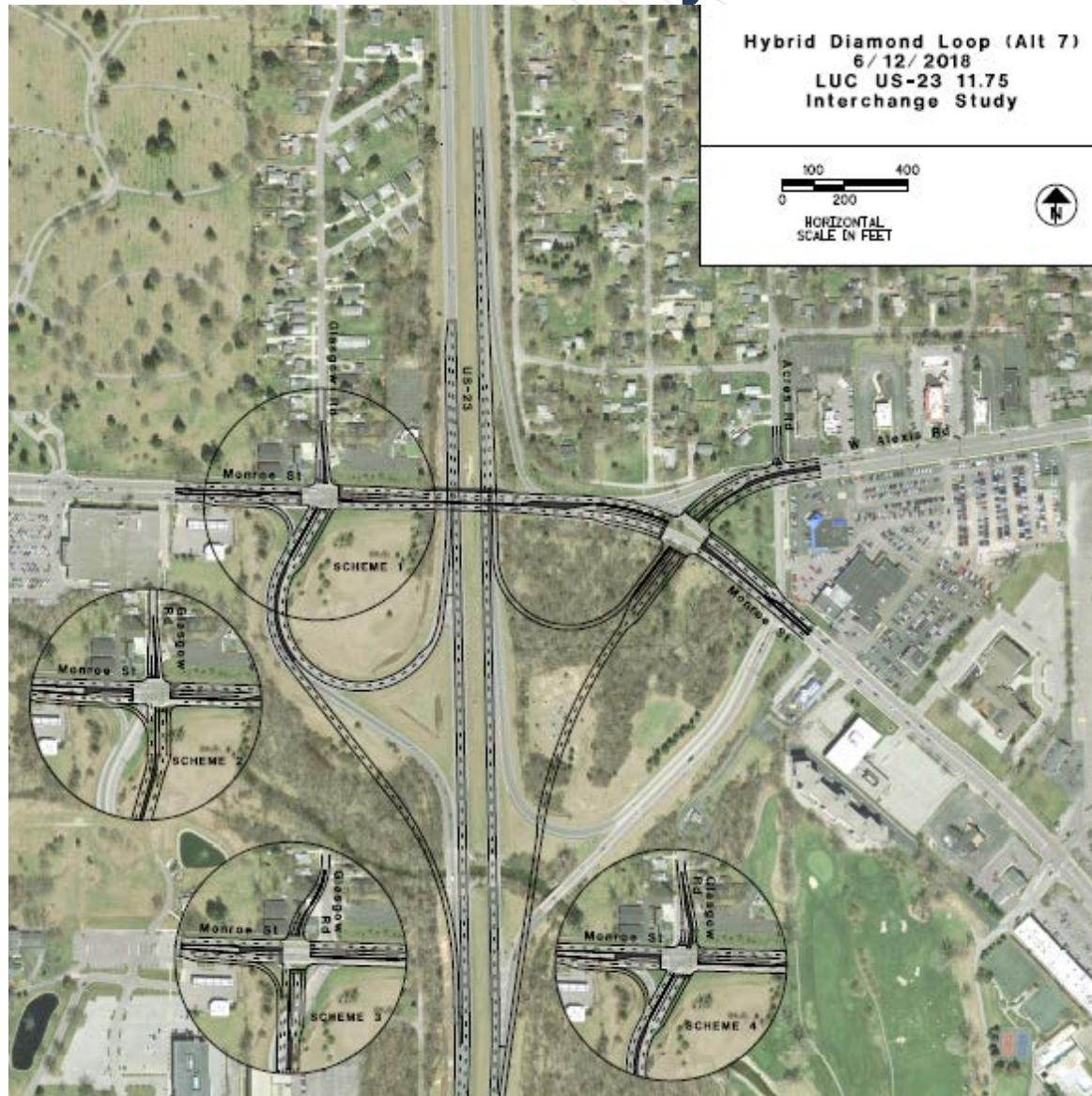
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Feasible Alternative – Alt 5 -Hybrid



Feasible Alternative – Alt 7 Hybrid



Next Steps

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- Completion of the Feasibility Study:
 - Public Meeting (September)
 - Completion of the Feasibility Study and ODOTs Approval
 - Identify project phasing
 - Identification of Funding Sources
 - Safety (Federal/ODOT)
 - Congestion Mitigation and Air Quality (CMAQ) (TMACOG)
 - TRAC Program (ODOT)
 - ODOT District Pavement & Bridge Preservation
 - ODOT Local Bridge Maintenance
 - ODOT Urban Paving Program
 - ODOT State Infrastructure Program
 - TMACOG Transportation Alternatives (TAP)
 - OPWC Grants
 - Lucas County TID
- TMACOG TIP Programming
- ODOT Programming