



Downtown Transportation Improvements Project

Initial Stakeholder Meeting
August 24, 2022

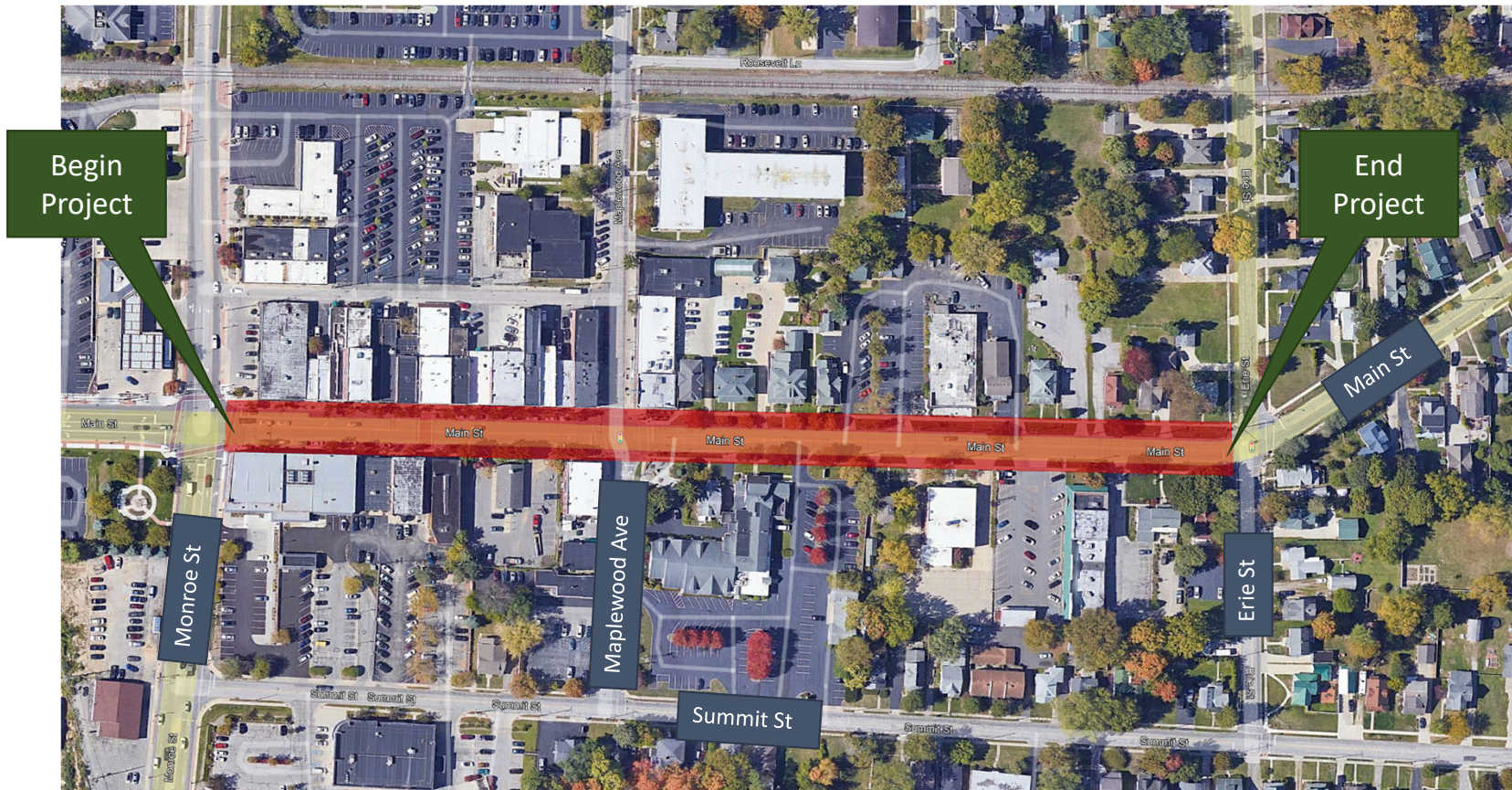


Introduction

- History
- Downtown Master Plan & Market Analysis 2020
- Survey 2013
- Revitalization in 1983 & 1987



Limits of Work



Anticipated Schedule

- Initial Stakeholder Meeting: August 24, 2022
- 2nd Stakeholder Meeting: January 2023
- Plan Development: 2023 thru September 2024
- Advertise & Award: September thru November 2024
- Begin Construction: April 2025



Primary Work Elements

- **Roadway Improvement**
 - Full depth pavement replacement
 - Parking
 - Sidewalks
 - ADA accessibility
 - Utility Improvement
 - Storm, water, sanitary
- **Aesthetics**
 - Architectural Elements
 - Landscape
 - Hardscape
- **Lighting, Technology**

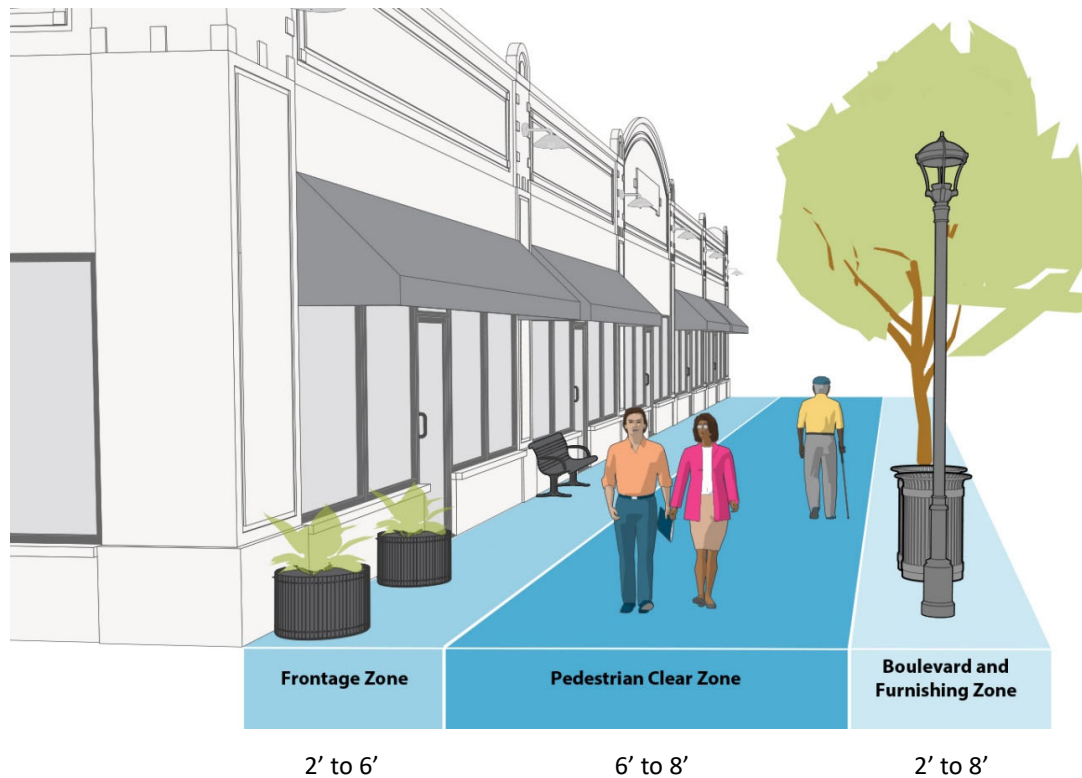


Roadway Elements

- **Lane widths**
 - Existing vs Proposed
- **Parking**
 - Current spots = 22
 - Angled, parallel, none
 - Additional locations (Maplewood to Erie)
- **Curb types**
 - 6" curb, no curb, bollards, striping
- **Community priorities**
 - Vehicular traffic, pedestrians, parking, frontage space



Sidewalk Elements



ADA Accessibility

- Storefront access
- Curb ramps
- Parking spots



Lighting & Technology (JDRM)

- **Street Lighting**
 - Streetlamps
 - Overhead string lights
- **Electrical Outlets**
- **WiFi**



Aesthetics (EDGE)

- Streetscapes make up the network of public spaces by which people move, socialize, and do business. This public realm provides critical opportunities to enhance the quality of life for all of those who live, work, and visit Downtown.
- Establishing a set of Downtown Streetscape Standards provides the City with a comprehensive set of standards that will address improvements within the right-of-way including sidewalks, curbs, crosswalks, street lighting, traffic signals, street furniture, landscaping, green infrastructure, public art, medians, outdoor dining, and parking lot screening.



Aesthetics (EDGE)

The composition of the streetscape should be Consistent along all Downtown streets. The following framework establishes the zones, rhythm and relationships that should guide the design of all streets.

GENERAL SIDEWALK ZONES

The streetscape is a system of zones between the street curb and the building facades. Each zone serves a functional purpose and requires minimum dimensions.

The Curb Zone

This zone is a 30" wide zone from the face of the vehicular curb to the nearest vertical element. Typically, the first vertical element will be a 4" high planter curb. The 30" dimension allows for parked vehicles to open passenger side doors without hitting a vertical element. It also allows those passengers to exit their vehicles and step onto a walking surface. The only permitted elements within the Curb Zone are parking meters and regulatory signs. These meters are located at the ends of parallel parking stalls and therefore should not be in the swing path of vehicular doors.



Aesthetics (EDGE)

The Amenity Zone

This zone contains most of the elements that define the character of the street. It contains the street lights and street trees which are critical to pedestrian safety and comfort in the urban environment. In addition, this zone includes curbed tree planters, tree grates (when necessary), and street furniture.

The Walk Zone

This zone is critical to pedestrian movements and must meet minimum dimensions for accessibility. In areas of higher pedestrian traffic, consideration should be given to wider Walk Zone widths.

The Building Zone

When the overall sidewalk width is sufficient, a Building Zone should be established against the building face. This zone allows for storefront door openings and potted plants to occur without impeding pedestrian flows in the Walk Zone. Building Zones that exceed 4'-0" in width allow for outdoor dining adjacent to the building.



Aesthetics (EDGE)

STREETSCAPE SECTIONS

typical cross sections

Minimum Dimensions

The minimum width for newly constructed streetscapes should be 12'-0". This dimension allows for a 3'-6" Amenity Zone which will accommodate a 2-1/2" caliper tree with a 2'-6" root ball in a curbed planter. In order to maximize long-term tree health, curbed plant beds are always recommended over tree grates.



Recommended Dimensions

A dimension of 15'-0" allows for 4" caliper trees with 3'-6" root balls to be planted in a 4'-6" (minimum) amenity zone. A 4" caliper tree will be more vandal resistant and will have a higher canopy height at the time of installation which will allow for better visibility to storefronts. In addition, this overall dimension allows for a Building Zone which complements the walk zone to create more comfortable pedestrian flows. This is also the minimum sidewalk width to accommodate a bus shelter. Fortunately, a large majority of existing Downtown streetscapes are made up of curb-to-building face dimensions that meet or exceed 15'-0".



Dimensions Greater Than 15-feet

Dimensions that exceed 15'-0" allow for greater flexibility in the width of the Walk Zone, Amenity Zone, and the Building Zone. If Walk Zone dimensions are sufficient for anticipated pedestrian flows, then curbed planters should be made wider to maximize tree root zones. Building Zones may be increased to accommodate larger outdoor dining areas but should not exceed 8'-0" in width.



Aesthetics (EDGE)

TYPICAL STREETSCAPE COMPOSITION

The rhythm of streetscape elements along a corridor begins with the spacing of parallel parking. Parking meters are typically located at the end of the parking spaces and should be accessible from the Walk Zone. Next, street lighting should be placed at the locations necessary to achieve the required illumination for vehicular traffic. The lights should be placed behind parking meters to allow street trees and curbed planters to occupy the spaces between parking meters. This establishes the basic rhythm of major elements within the Amenities Zone.

Intersections throughout Downtown should also be treated consistently. When required, traffic signals and curb ramps should be located first and conform to all required traffic control standards. Specialty pavements should be used to identify the pedestrian corners and provide a visual cue to pedestrians of the crosswalks and curb ramps. This pavement should also be utilized in the pedestrian crosswalks.



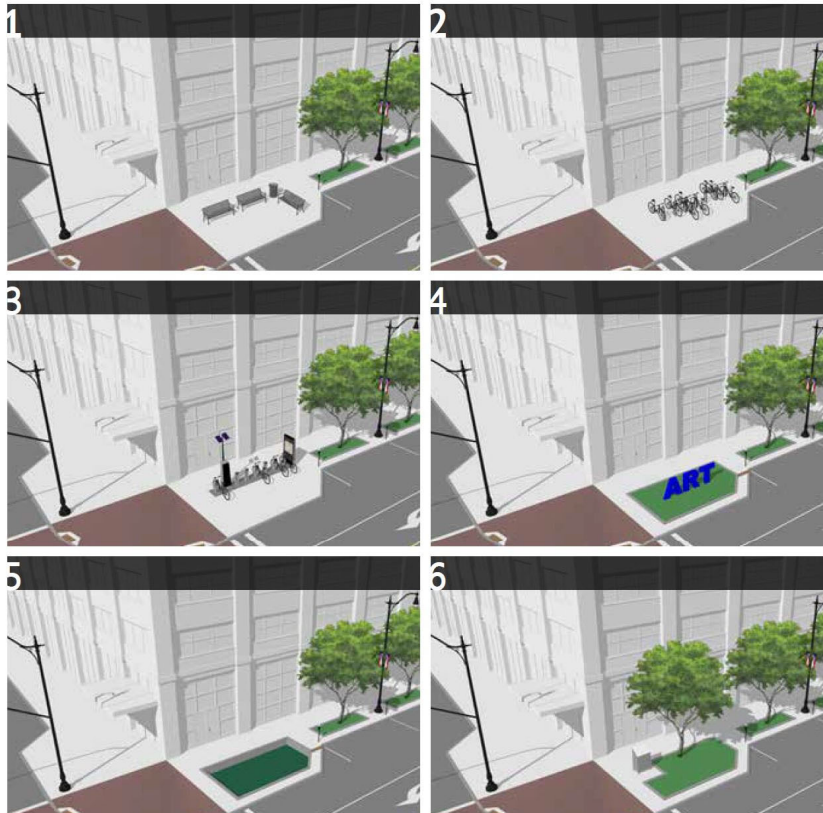
Aesthetics (EDGE)

When parallel parking exists, and is not limited by restricted hours, pedestrian bump outs should be utilized to minimize pedestrian crossing distances and to create additional areas for pedestrian queuing and/or amenities within the streetscape. These areas can be used for a variety of elements including the following:

1. Furniture (including benches and trash receptacles)
2. Bike racks
3. Bike Share Stations
4. Public art
5. Green Infrastructure
6. Utility infrastructure with screening. Refer to City of Columbus Traffic Signal Design Manual for Downtown.

Elements located in the bump out areas shall not interfere with safe vehicular sight lines.

The compositional framework described in this chapter illustrates the desired function and general spatial relationships of all Downtown streetscapes. The following chapter provides more detailed guidance for the specific placement and material specifications for all streetscape elements.



Aesthetics (EDGE)

SIDEWALKS

Sidewalks shall be gray concrete with a light to medium broom finish or buff wash finish. Sidewalks at corners, including the crosswalk, shall be brick with concrete containment banding. Detectable warning tiles shall be cast iron or granite.

placement guidelines

- Install concrete and brick per COC Standards.
- Install clay brick on bituminous setting bed on concrete base per COC Standards.
- Meet all ADA and COC Standards for cross slopes and curb ramps.
- Install ADA detectable warning plate per COC Standards.

material standard

Sidewalk

- Concrete, light to medium broom finish, perpendicular to traffic flow, 5'-0" x 5'-0" minimum joint pattern, generally square, with 1/4" wide saw cut joints. A buff wash finish may be substituted, but a consistent finish should be used between public street intersections.

Pedestrian Pavement at Intersection Corners

- Pine Hall Brick "English Edge Full Range", 8"x4"x2-1/4" minimum thickness, or approved equal.

Crosswalk Pavement

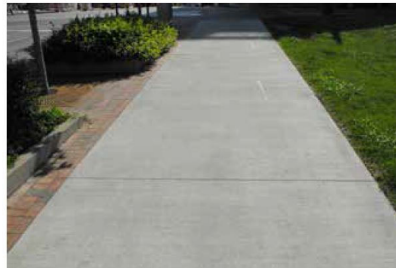
- Pine Hall Brick "English Edge Full Range", 8"x4"x2-3/4" minimum thickness, or approved equal.

Detectable Warning Plate (cast iron)

- Neenah Detectable Warning Plate, 24"x24", East Jordan Iron Works Detectable Warning Plate.

Detectable Warning Paver (granite)

- Wausau Red or Radient Red, or approved equal.



The final step in designing a streetscape is the selection of the specific elements and materials. The Streetscape Plan will provide detailed information on the standard elements and materials that make up the Downtown streetscape.



Aesthetics (EDGE)

STREET CURBS

Street curbs shall be granite for long-term durability and aesthetic value.

placement guidelines

- Install granite curb per City of Columbus Standards.

material standard

High Street

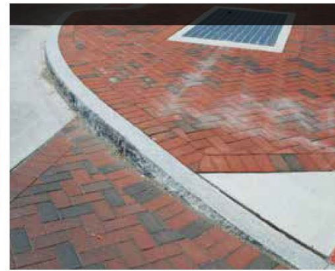
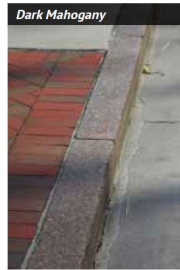
- Dark Mahogany Granite

Downtown Distributors

- Mount Airy Granite
- Downtown Distributors include Mound Street and Fulton Street (east of Third Street), Lester Drive and Elijah Pierce Avenue.

All Remaining Streets

- Caledonia Granite
- Mound Street and Fulton Street in River South (west of Third Street).



Aesthetics (EDGE)

- Trees, plantings
- Color and variation possibilities
- Sightlines and physical obstruction
- Hardiness
- Maintenance



Roadway Concepts

- **7 Preliminary Concepts**
 - Concept A: Parallel Both Sides
 - Concept B: Parallel One Side
 - Concept C: Angled One Side
 - Concept D: Angled Both Sides
 - Concept E: Angled and Parallel
 - Concept F: No Curb with Median Drain

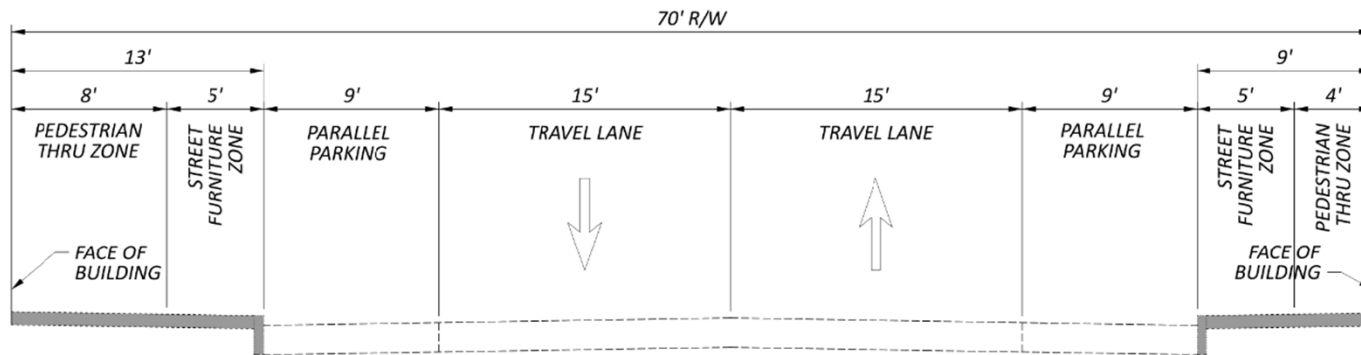


Key Components

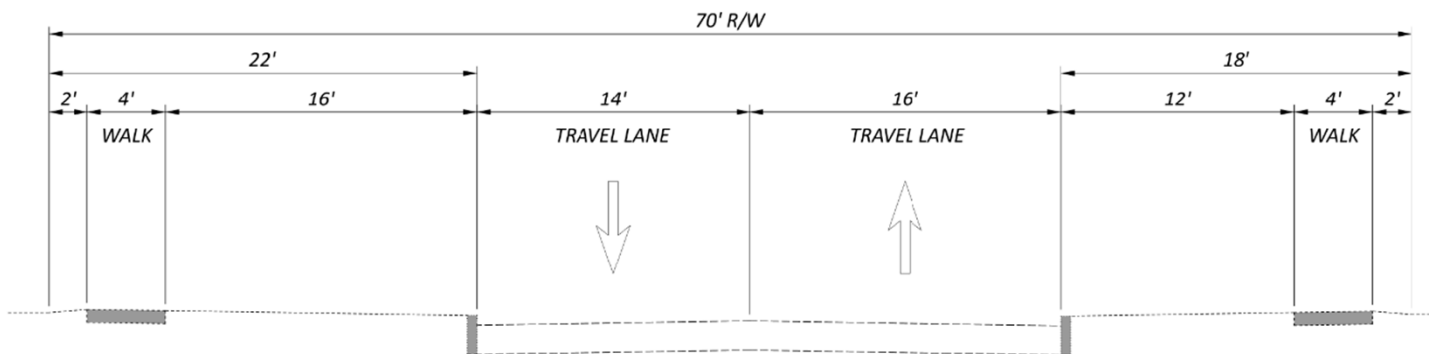
- Parking Types
- Amount of Parking
- Sidewalk Width
- Space for Street Furnishings



Existing Roadway

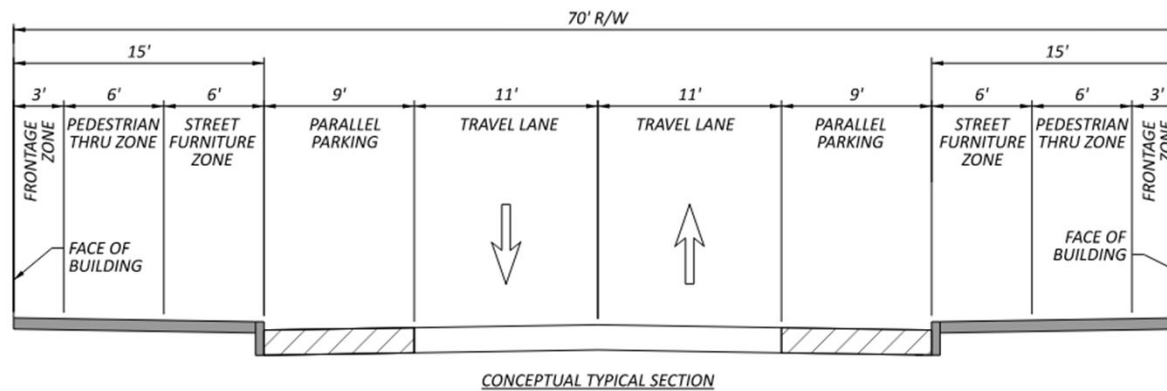
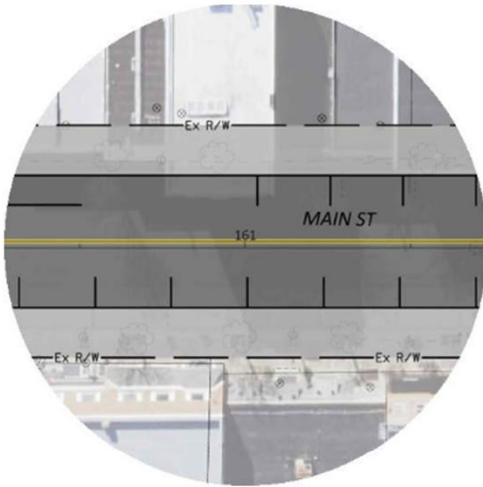


EXISTING TYPICAL SECTION
FROM MONROE ST TO MAPLEWOOD AVE

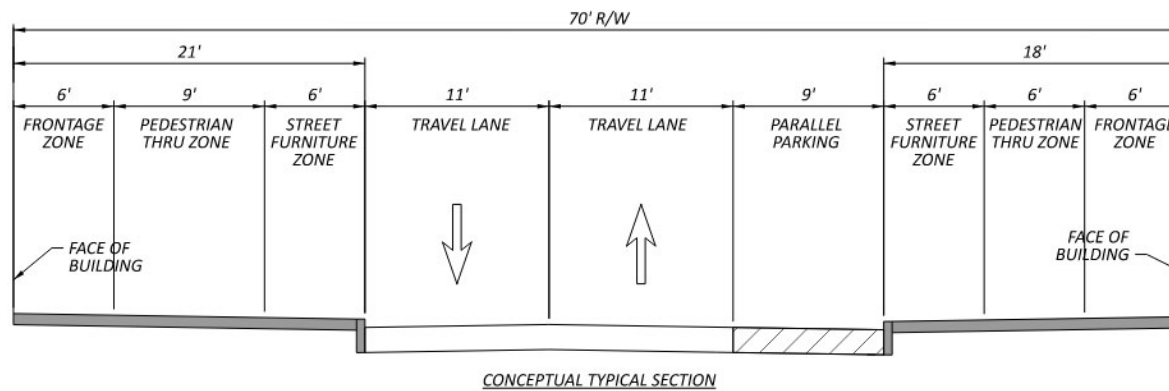
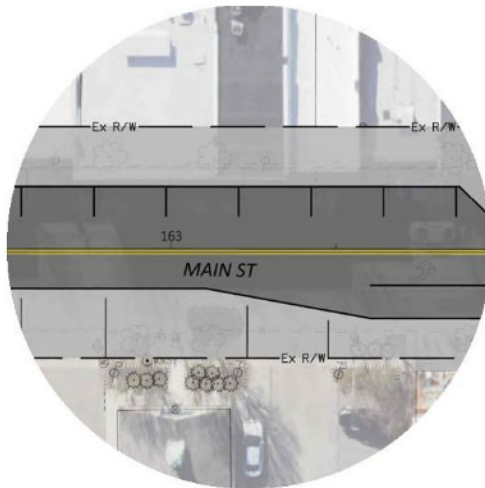


EXISTING TYPICAL SECTION
FROM MAPLEWOOD AVE TO ERIE ST

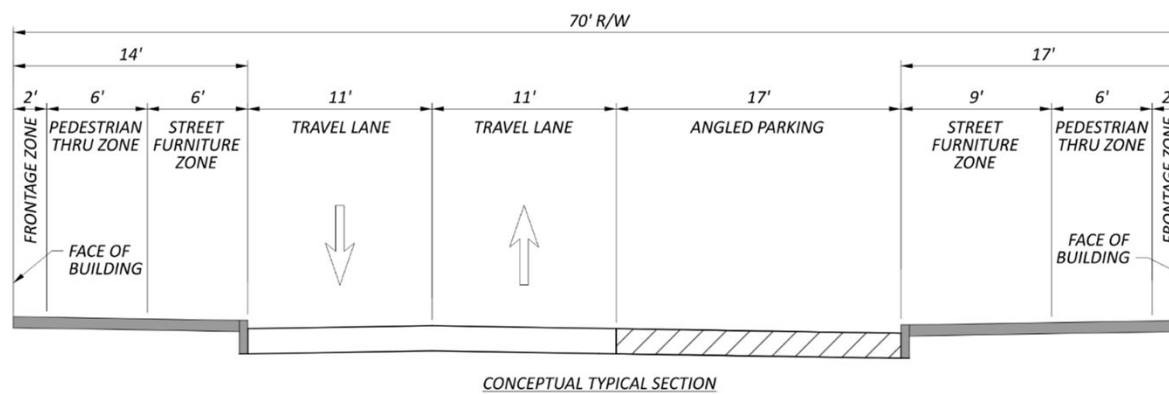
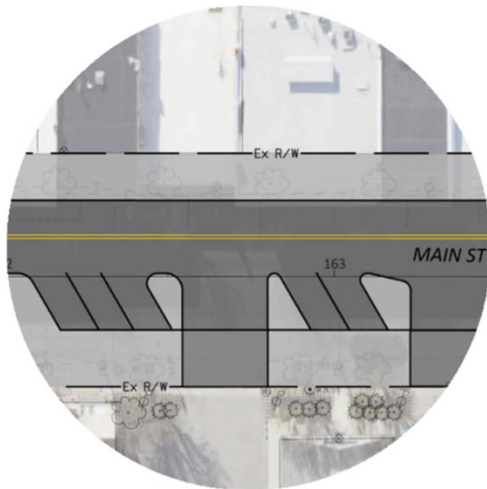
Concept A *(Parallel Parking Both Sides)*



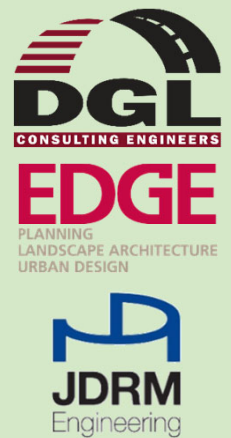
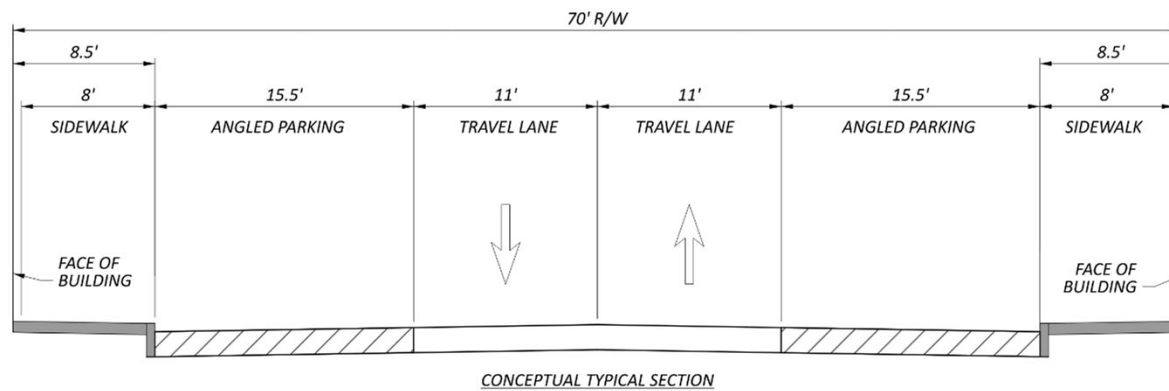
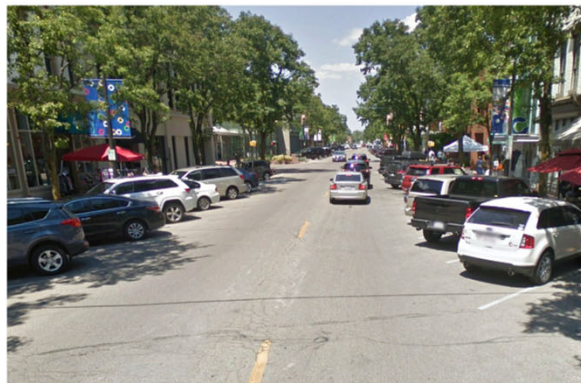
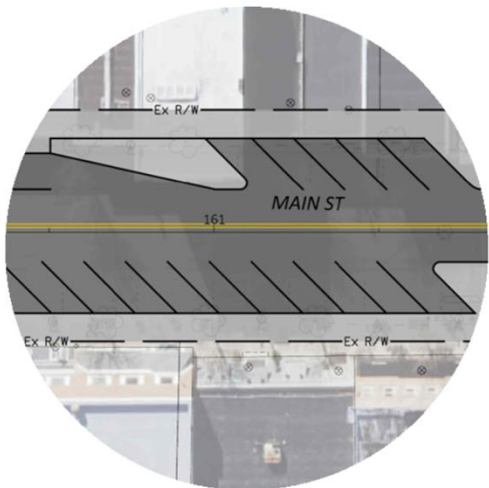
Concept B *(Parallel Parking One Side)*



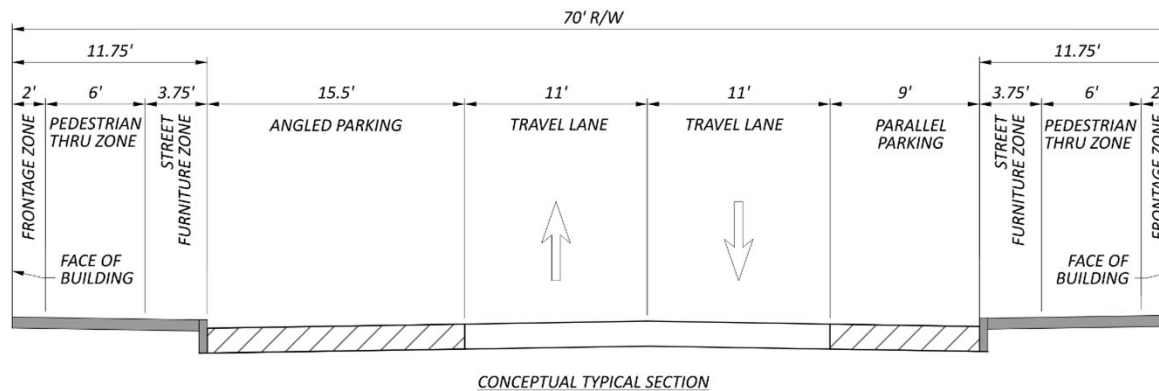
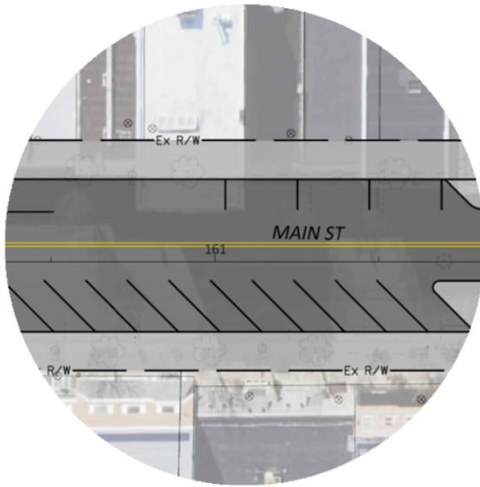
Concept C *(Angled Parking One Side)*



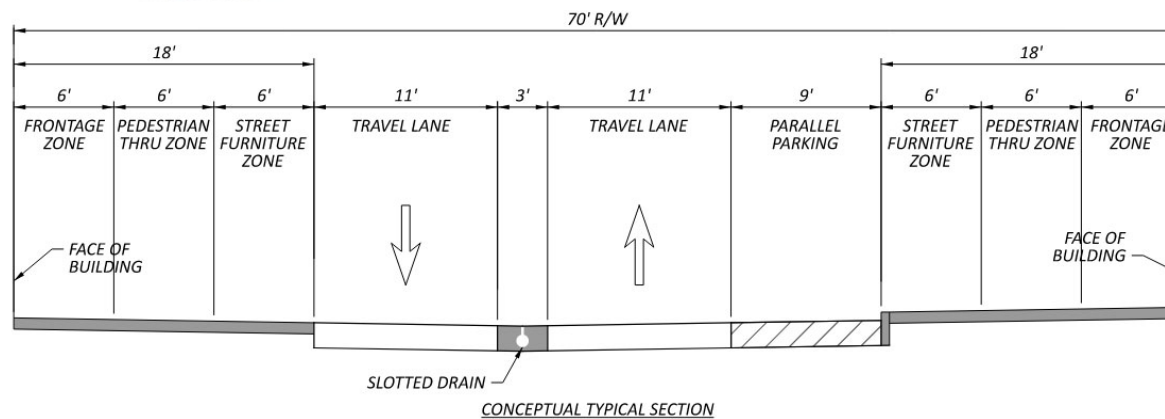
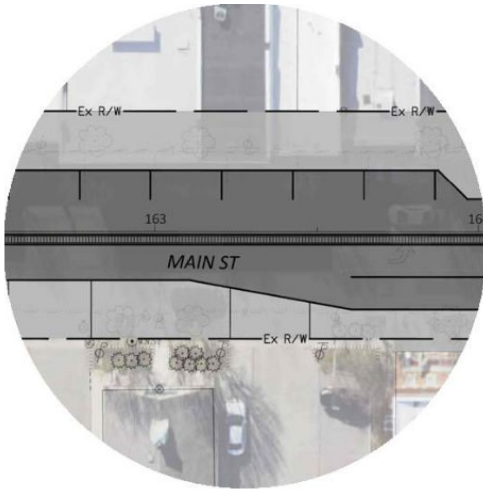
Concept D *(Angled Parking Both Sides)*



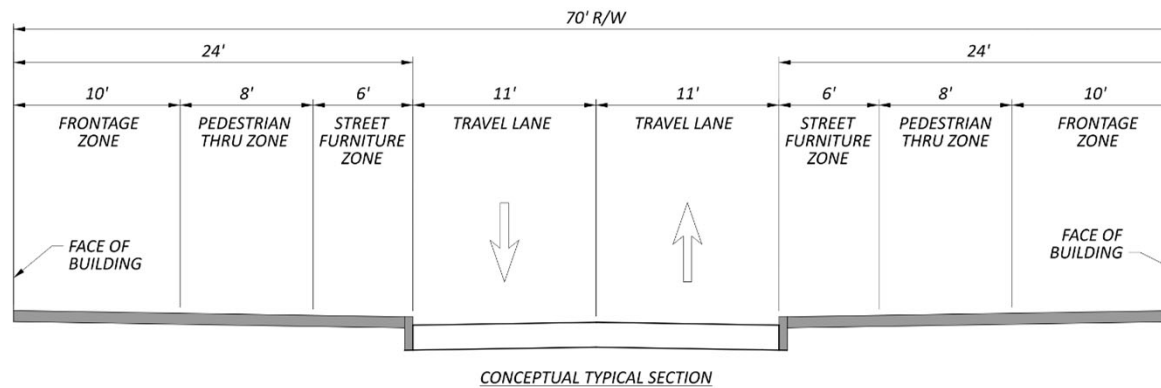
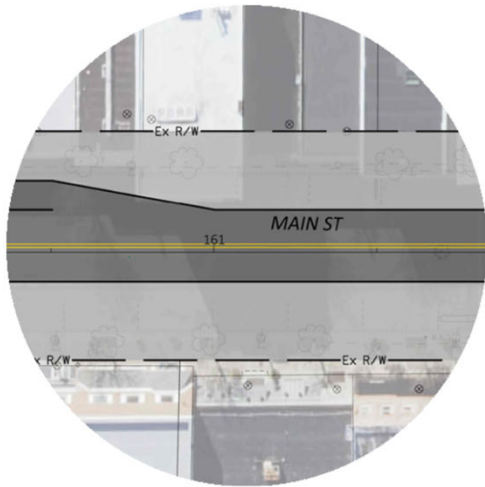
Concept E *(Angled and Parallel Parking)*



Concept F *(No Curb with Median Slotted Drain)*



Concept G (No Parking)





THANK YOU

